AVON PARKII

ZONING MAP AMENDMENT PLAN

GENERAL NOTES:

WNER: BELLEVUE REAL ESTATE, L.L.C. 16 PINEY GLEN CT.

POTOMAC, MD 20854 AGENT: VITO CETTA

DEVELOPER/CONTACT:

DICKERSON HOMES LLC
925 E. JEFFERSON ST, SUITE B

CHARLOTTESVILLE, VA 22902
CONTACT: BEAU DICKERSON

CLAY GREEN CONSULTING 1830 RIVER INN LANE CHARLOTTESVILLE, VA 22901 CONTACT: CLAY GREEN

PLAN PREPARER: ROUDABUSH, GALE & ASSOCIATES 914 MONTICELLO ROAD

CHARLOTTESVILLE, VA 22902

TAX MAP PARCEL NO: 09000-00-03100 (D.B. 3786 P.G. 060)

PROPERTY ADDRESS: 1960 AVON STREET EXT. CHARLOTTESVILLE, VA 22902

ZONING: PRD PLANNED RESIDENTIAL DEVELOPMENT PER ZMA 20120004

WITH ENTRANCE CORRIDOR OVERLAY (EC)

EXISTING USE: RESIDENTIAL PROPERTY SIZE: 5.262 ACRES

MAGISTERIAL DISTRICT: SCOTTSVILLE DISTRICT

SETBACKS: VARY - REFER TO LAYOUT PLAN

35 FEET

TOTAL NUMBER OF UNITS: 32

MAX. BUILDING HEIGHT:

PROPOSED DENSITY: 6.0 DU/AC

PROPOSED USE: RESIDENTIAL

DRAINAGE DISTRICT: RIVANNA RIVER

WATERSHED: THIS SITE IS NOT LOCATED WITHIN THE WATERSHED OF A PUBLIC

WATER SUPPLY RESERVOIR.

SURVEY: BOUNDARY, TOPOGRAPHIC AND GROUND SURVEY BY LINCOLN SURVEYING DATED MAY 28, 2010. NAVD '88 VERTICAL DATUM

(BENCHMARK IS SURVEY CONTROL POINT T-5000, NAIL AND FLASHER

ELEVATION 300.27)

FLOODPLAIN: THIS PROPERTY LIES IN AN AREA DESIGNATED AS ZONE X AS SHOWN ON THE FLOOD INSURANCE RATE MAP COMMUNITY PANEL NO:

51003C0407D, DATED FEBRUARY 4, 2005.

CRITICAL SLOPES: THERE ARE MANAGED STEEP SLOPES LOCATED ON ALBEMARLE GIS FOR THIS PARCEL; ASBUILT FIELD CONDITIONS INDICATE THAT

THERE ARE NO SLOPES GREATER THAN 25%. SEE EXISTING

CONDITIONS SHEET.

OTHER: THERE ARE NO WETLANDS OR HISTORIC FEATURES EXISTING

ON THE SUBJECT PROPERTY.

MAXIMUM IMPERVIOUS COVER: THE OVERALL SITE IMPERVIOUS COVER PROPOSED SHALL BE UNDER 37% OF IMPERVIOUS COVER.

3/% OF IMPERVIOUS COVER.

STORMWATER MANAGEMENT: UNDERGROUND INFILTRATION WITH AN EXTENDED DETENTION POND WILL BE PROVIDED TO OFFSET THE POST—DEVELOPED

STORMWATER IMPACTS FROM THIS SITE. DETENTION SHALL BE PROVIDED TO ELIMINATE THE NEED FOR ANY OFFSITE DRAINAGE

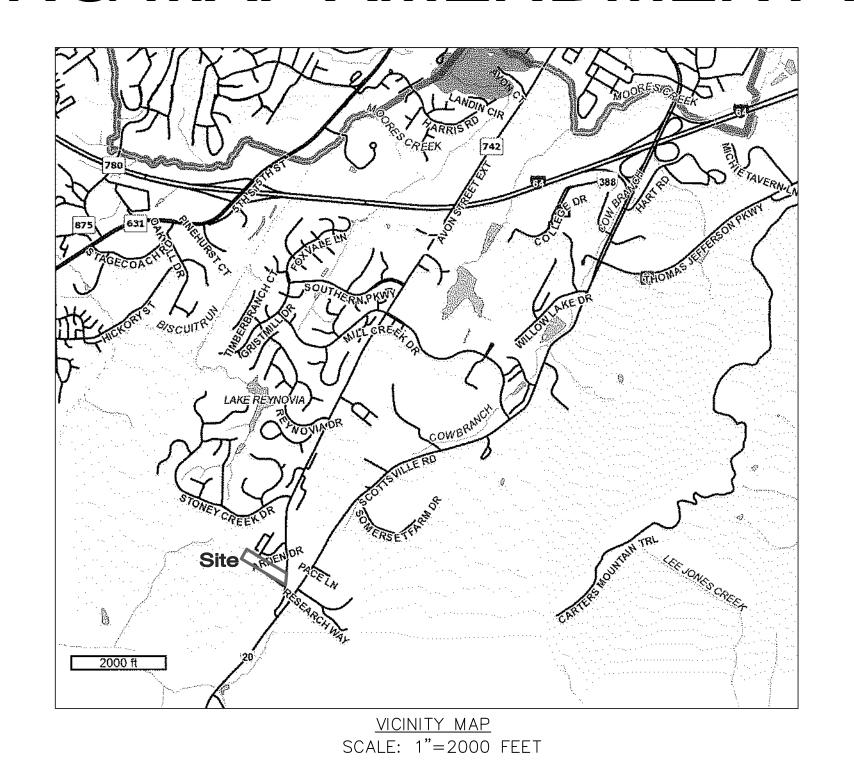
APROVEMENT

PARKING SCHEDULE:

LOT	UNIT TYPE	SPACES REQUIRED	LOT SPACES PROVIDED	ONSTREET PARKING
1-7	SFD W/ 2 OFFSTREET SPACES	14 (2 SPACES PER UNIT)	14	
8-15	SFD W/2 OFFSTREET SPACES	16 (2 SPACES PER UNIT)	16	'No Parking'
16-22	SFD W/ 2 OFFSTREET SPACES	14 (2 SPACES PER UNIT)	14	
23-26	3 BR TOWNHOUSE	9 2 SPACES PER UNIT & 1 SPACE PER 4 UNITS)	0	11
27-30	3 BR TOWNHOUSE	9 2 SPACES PER UNIT & 1 SPACE PER 4 UNITS)	0	11
HATHAWAY		EXIST. ONE—SIDED PARKING		
SHARED PARKING				10
SUBTOTALS		62 SPACES REQUIRED	44	<u>32</u>
TOTAL PARKING PROVIDED			76 TOTAL	<u>SPACES</u>

TRIP GENERATION

CODE	LAND USE	AVG. WEEKDAY TRIPS		ADJ. STREET PEAKS (AM)		ADJ. STREET PEAKS (PM)		SATURDAY		SUNDAY	
		PER UNIT	PROJECT	PER UNIT	PROJECT	PER UNIT	PROJECT	PER UNIT	PROJECT	PER UNIT	PROJECT
210	22 SFD UNITS	9.57	210	0.75	17	1.01	22	10.1	222	8.87	195
230	8 TH/APT UNITS	5.86	47	.44	4	.52	4	5.67	45	4.84	39
TOTAL	TRIPS GENERATED		257		20		26		267		234



APPROVALS	DATE
DEPARTMENT OF COMMUNITY DEVELOPMENT	
PLANNING / ZONNING	
ENGINEERING	
INSPECTIONS	
ARCHITECTUAL REVIEW BOARD	
FIRE RESCUE	
ALBEMARLE COUNTY SERVICE AUTHORITY	
VA. DEPARTMENT OF TRANSPORTATION	

OPEN SPACE CALCULATIONS:

TOTAL SITE AREA	229,211 S.F.		
OPEN SPACE REQUIRED	57,303 S.F. (25%)		
OPEN SPACE PROVIDED	65,428 S.F. (28.5%)		

AREA CALCULATIONS:

TOTAL SITE AREA	5.26 AC. (100%)
LOTS	2.91 AC. (55.3%)
PUBLIC RIGHT OF WAY	0.43 AC. (8.1%)
PRIVATE RIGHT OF WAY	0.40 AC. (7.6%)
OPEN SPACE	1.50 AC. (29%)



SHEET INDEX

SHEET 1 ----- COVER SHEET

SHEET 2 ----- OVERALL LAYOUT & DRAINAGE PLAN

SHEET 3 ----- EXISTING CONDITIONS

SHEET 4 ----- EXISTING CONDITIONS

SHEET 5 ----- LAYOUT PLAN

SHEET 6 ----- LAYOUT PLAN

SHEET 7 ----- GRADING AND UTILITY PLAN

SHEET 8 ----- CONCEPTUAL BMP AND GRADING PLAN

SHEET 9 ----- LANDSCAPING PLAN

SHEET 10 ----- LANDSCAPING PLAN

SHEET 10A ----- LINE OF SIGHT EXHIBIT

GENERAL CONSTRUCTION NOTES

- 1. PRIOR TO ANY CONSTRUCTION WITHIN ANY EXISTING PUBLIC RIGHT-OF-WAY, INCLUDING CONNECTION TO ANY EXISTING ROAD, A PERMIT SHALL BE OBTAINED FROM THE VIRGINIA DEPARTMENT OF TRANSPORTATION (VDOT). THIS PLAN AS DRAWN MAY NOT ACCURATELY REFLECT THE REQUIREMENTS OF THE PERMIT. WHERE ANY DISCREPANCIES OCCUR THE REQUIREMENT OF THE PERMIT SHALL GOVERN.
- 2. ALL MATERIALS AND CONSTRUCTION METHODS SHALL CONFORM TO CURRENT SPECIFICATIONS AND STANDARDS OF VDOT UNLESS OTHERWISE NOTED.
- 3. EROSION AND SILTATION CONTROL MEASURES SHALL BE PROVIDED IN ACCORDANCE WITH THE APPROVED EROSION CONTROL PLAN AND SHALL BE INSTALLED PRIOR TO ANY CLEARING, GRADING OR OTHER CONSTRUCTION.
- 4. ALL SLOPES AND DISTURBED AREAS ARE TO BE FERTILIZED, SEEDED, AND MULCHED.
- 5. THE MAXIMUM ALLOWABLE SLOPE IS 2:1 (HORIZONTAL: VERTICAL). WHERE REASONABLY OBTAINABLE, LESSER SLOPES OF 3:1 OR BETTER ARE TO BE ACHIEVED.
- 6. PAVED, RIP—RAP OR STABILIZATION MAT LINED DITCH MAY BE REQUIRED WHEN IN THE OPINION OF COUNTY ENGINEER, OR DESIGNEE, IT IS DEEMED NECESSARY IN ORDER TO STABILIZE A DRAINAGE CHANNEL.
- 7. TRAFFIC CONTROL SIGNS SHALL CONFORM TO THE VIRGINIA MANUAL FOR UNIFORM TRAFFIC CONTROL DEVICES.
- 8. UNLESS OTHERWISE NOTED, ALL CONCRETE PIPES SHALL BE REINFORCED CONCRETE PIPE—CLASS III.
- 9. ALL EXCAVATION FOR UNDERGROUND PIPE INSTALLATION MUST COMPLY WITH OSHA STANDARDS FOR CONSTRUCTION INDUSTRY (29 CFR PART 1926).

GENERAL CONSTRUCTION NOTES FOR STREETS

- 1. CONSTRUCTION INSPECTION OF ALL PROPOSED ROADS WITHIN THE DEVELOPMENT WILL BE MADE BY THE COUNTY. THE CONTRACTOR MUST NOTIFY THE DEPARTMENT OF COMMUNITY DEVELOPMENT (296-5832) 48 HOURS IN ADVANCE OF THE START OF CONSTRUCTION.
- 2. UPON COMPLETION OF FINE GRADING AND PREPARATION OF THE ROADBED SUBGRADE, THE CONTRACTOR SHALL HAVE CBR TESTS PERFORMED ON THE SUBGRADE SOIL. THREE (3) COPIES OF THE TEST RESULTS SHALL BE SUBMITTED TO THE COUNTY IF A SUBGRADE SOIL CBR OF 10 OR GREATER IS NOT OBTAINABLE, A REVISED PAVEMENT DESIGN SHALL BE MADE BY THE DESIGN ENGINEER AND SUBMITTED WITH THE TEST RESULTS FOR APPROVAL.
- 3. SURFACE DRAINAGE AND PIPE DISCHARGE MUST BE RETAINED WITHIN THE PUBLIC RIGHT-OF-WAY OR WITHIN EASEMENTS PRIOR TO ACCEPTANCE BY THE COUNTY. ALL DRAINAGE OUTFALL EASEMENTS ARE TO BE EXTENDED TO A BOUNDARY LINE OR A NATURAL WATERCOURSE.
- 4. GUARDRAIL LOCATIONS ARE APPROXIMATE. EXACT LENGTH, LOCATION, AND APPROPRIATE END TREATMENTS WILL BE FIELD VERIFIED AT THE TIME OF CONSTRUCTION. ADDITIONAL GUARDRAIL MAY BE REQUIRED AT LOCATIONS NOT SHOWN WHEN, IN THE OPINION OF THE COUNTY ENGINEER, OR DESIGNEE, IT IS DEEMED NECESSARY. WHEN GUARDRAIL IS REQUIRED, IT SHALL BE INSTALLED FOUR (4) FEET OFFSET FROM THE EDGE OF PAVEMENT TO THE FACE OF GUARDRAIL, AND ROADWAY SHOULDER WIDTHS SHALL BE INCREASED TO SEVEN (7) FEET.
- 5. WHERE URBAN CROSS SECTIONS ARE INSTALLED, ALL RESIDENTIAL DRIVEWAY ENTRANCES SHALL CONFORM TO VDOT CG-9(A, B, OR C).
- 6. WHERE RURAL CROSS SECTIONS ARE INSTALLED, ALL RESIDENTIAL DRIVEWAY ENTRANCES SHALL CONFORM TO VDOT STANDARD PE-1.
- 7. COMPLIANCE WITH THE MINIMUM PAVEMENT WIDTH, SHOULDER WIDTH, AND DITCH SECTIONS, AS SHOWN ON THE TYPICAL PAVEMENT SECTION DETAIL, SHALL BE STRICTLY ADHERED TO.
- 8. ROAD PLAN APPROVAL FOR SUBDIVISIONS IS SUBJECT TO FINAL SUBDIVISION PLAT VALIDATION. SHOULD THE FINAL PLAT FOR THIS PROJECT EXPIRE PRIOR TO SIGNING AND RECORDATION, THEN APPROVAL OF THESE PLANS SHALL BE NULL AND VOID.
- 9. ALL SIGNS OR OTHER REGULATORY DEVICES SHALL CONFORM WITH THE VIRGINIA MANUAL FOR UNIFORM TRAFFIC CONTROL DEVICES AND THE ALBEMARLE COUNTY ROAD NAMING AND PROPERTY NUMBERING ORDINANCE AND MANUAL.
- 10. TRAFFIC CONTROL OR OTHER REGULATORY SIGNS OR BARRICADES SHALL BE INSTALLED BY THE DEVELOPER WHEN, IN THE OPINION OF THE COUNTY ENGINEER, OR DESIGNEE, THEY ARE DEEMED NECESSARY IN ORDER TO PROVIDE SAFE AND CONVENIENT ACCESS.
- 11. THE SPEED LIMITS TO BE POSTED ON SPEED LIMIT SIGNS ARE TO BE 5 MPH BELOW THE DESIGN SPEED, OR AS DETERMINED BY VDOT FOR PUBLIC ROADS.
- 12. VDOT STANDARD CD-1 OR CD-2 CROSS-DRAINS ARE TO BE INSTALLED UNDER THE SUBBASE MATERIAL AT ALL CUT AND FILL TRANSITIONS AND GRADE SAG POINTS AS SHOWN ON THE ROAD PROFILES.
- 13. A VIDEO CAMERA INSPECTION IS REQUIRED FOR ALL STORM SEWERS AND CULVERTS THAT ARE DEEMED INACCESSIBLE TO VDOT OR COUNTY INSPECTIONS. THE VIDEO INSPECTION SHALL BE CONDUCTED IN ACCORDANCE WITH VDOT'S VIDEO CAMERA INSPECTION PROCEDURE AND WITH A VDOT OR COUNTY INSPECTOR PRESENT.

SH, GALE & ASSOCIATES, IN SURVEYORS AND LAND PLANNERS FESSIONAL CORPORATION SINGE 1956

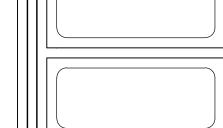
REVISIONS
July 03, 2015
ZMA Comments
Jan. 12, 2016
March 07, 2016
ZMA Comments
March 29, 2016

Add Sheet 10A

May 24, 2016

ZMA Comments

DATE Feb. 10, 2015

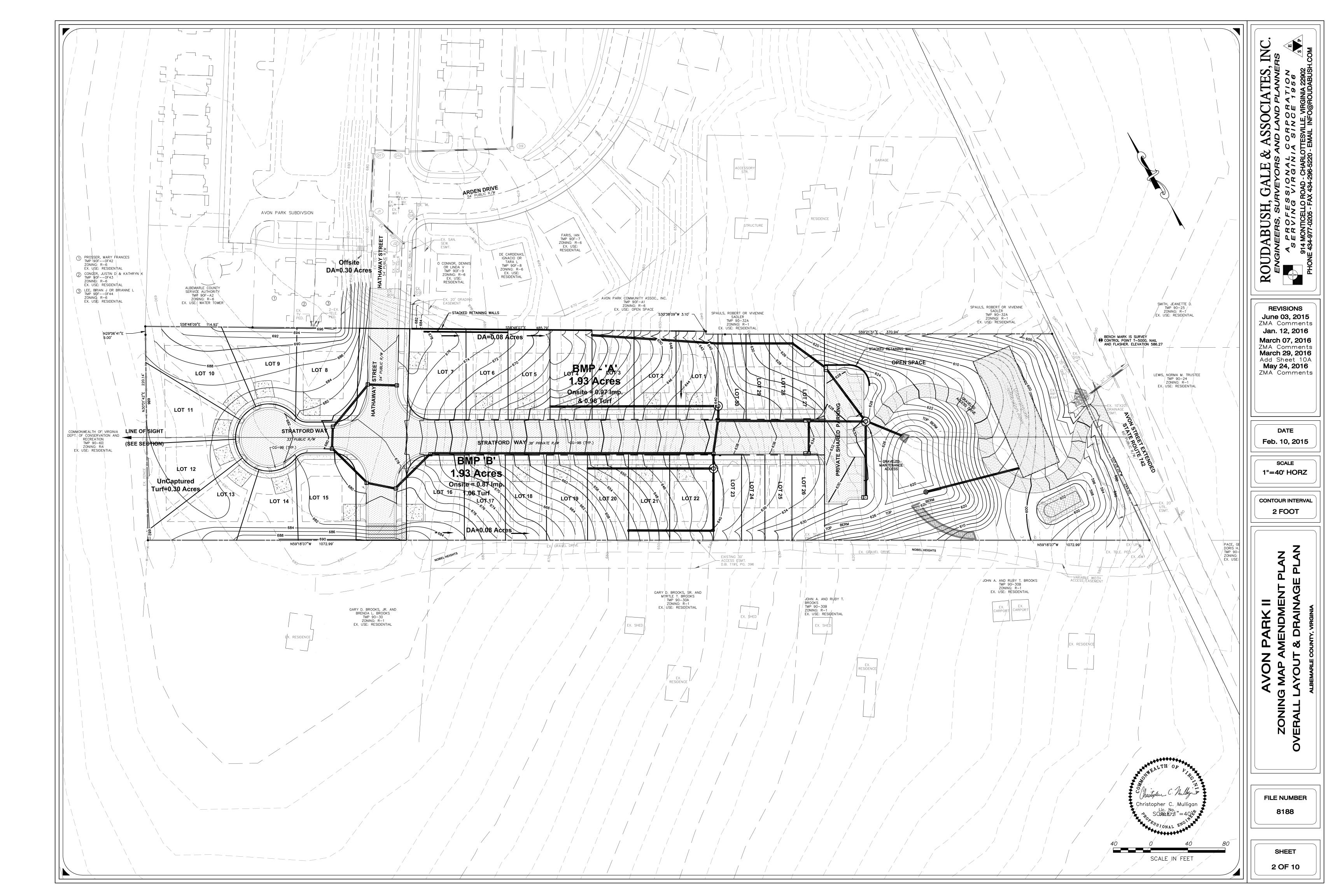


AVON PARK II
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COVER SHEET

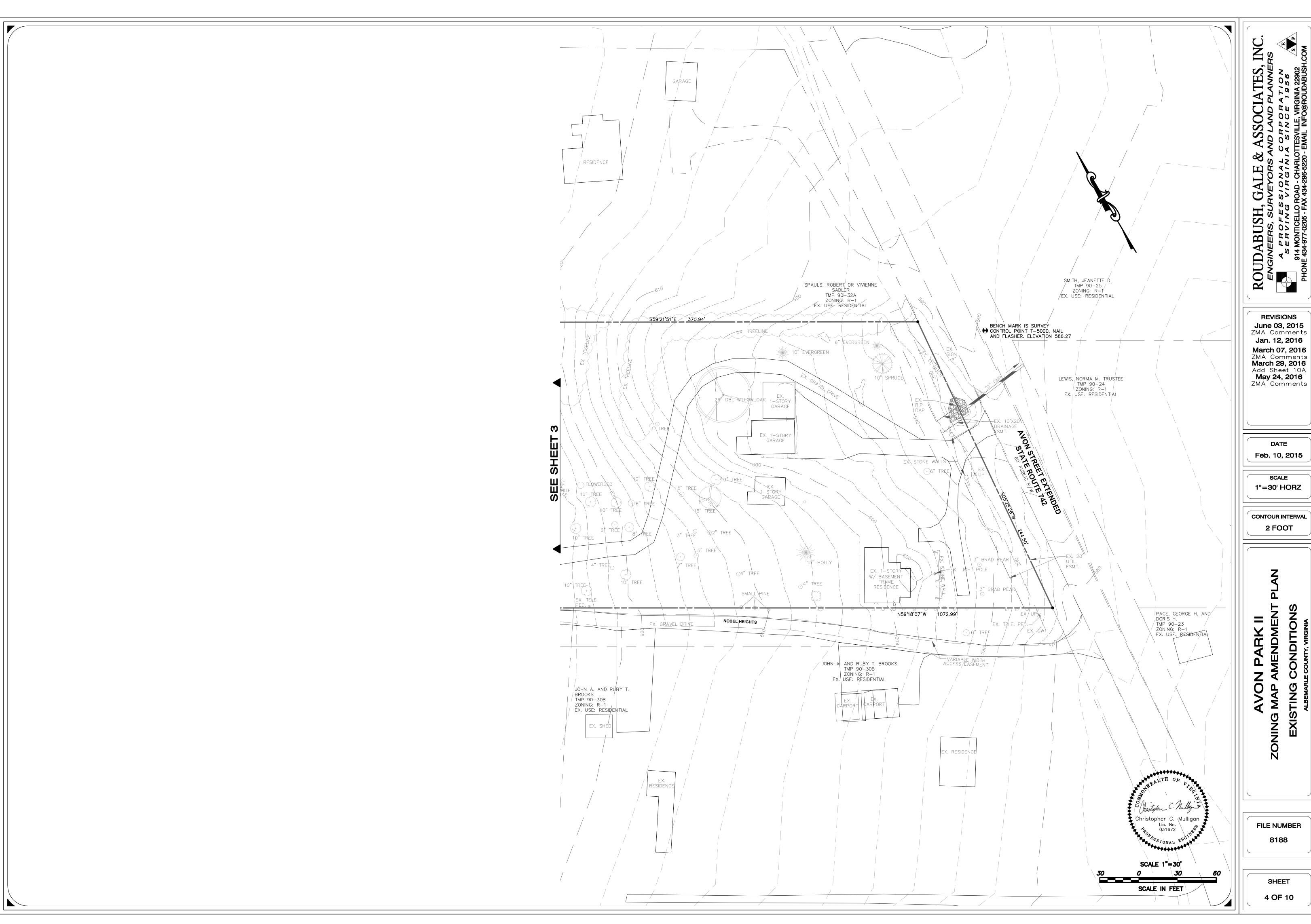
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SHEET

1 OF 10







ROUDABUSH, (
ENGINEERS, SUR'

A PROFESS

914 MONTICELLO R

PHONE 434-977-0205 - FAX **REVISIONS**

DATE Feb. 10, 2015

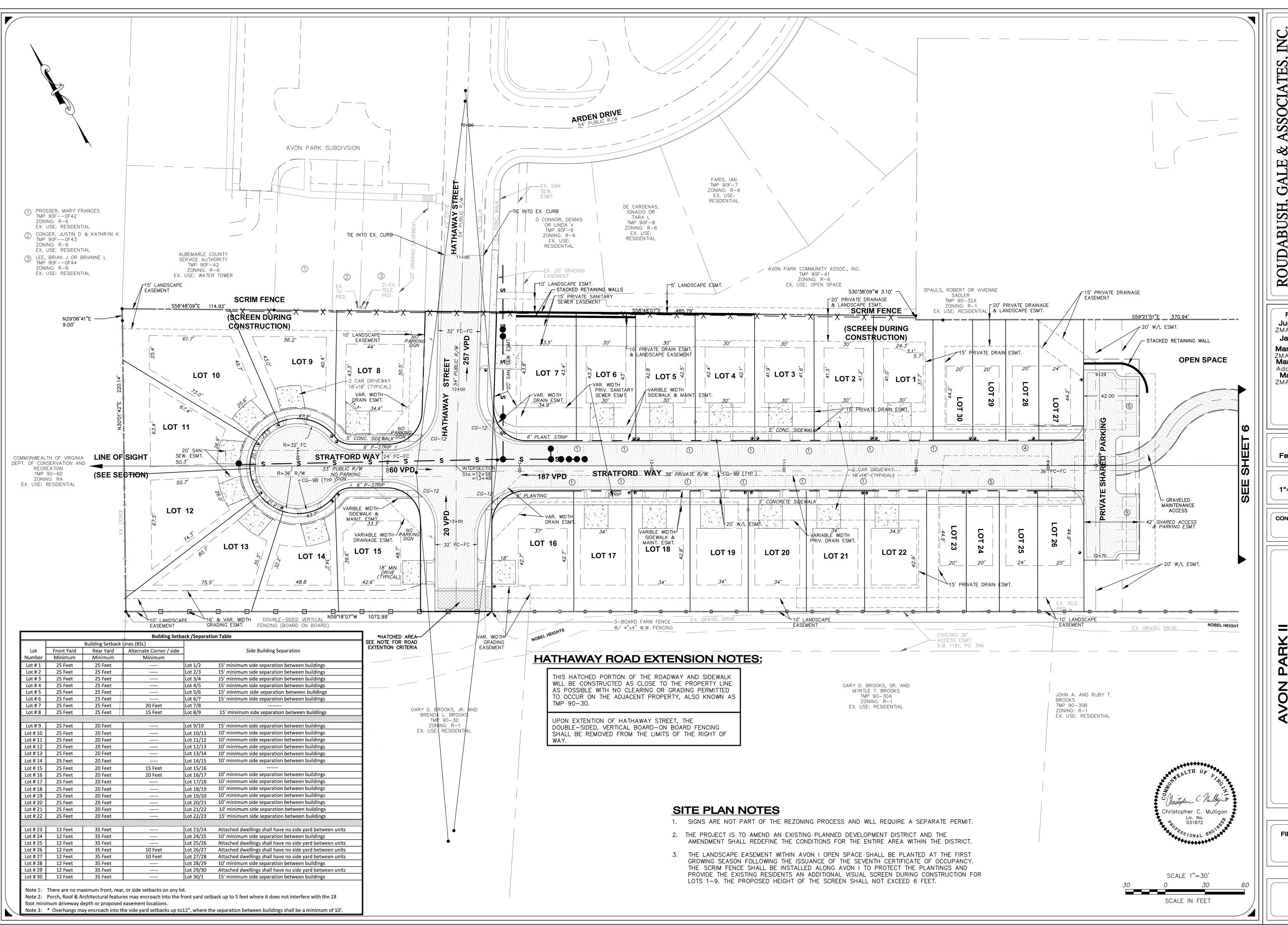
SCALE

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CONTOUR INTERVAL

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REVISIONS June 03, 2015 ZMA Comments Jan. 12, 2016 March 07, 2016 ZMA Comments March 29, 2016 Add Sheet 10A May 24, 2016 ZMA Comments

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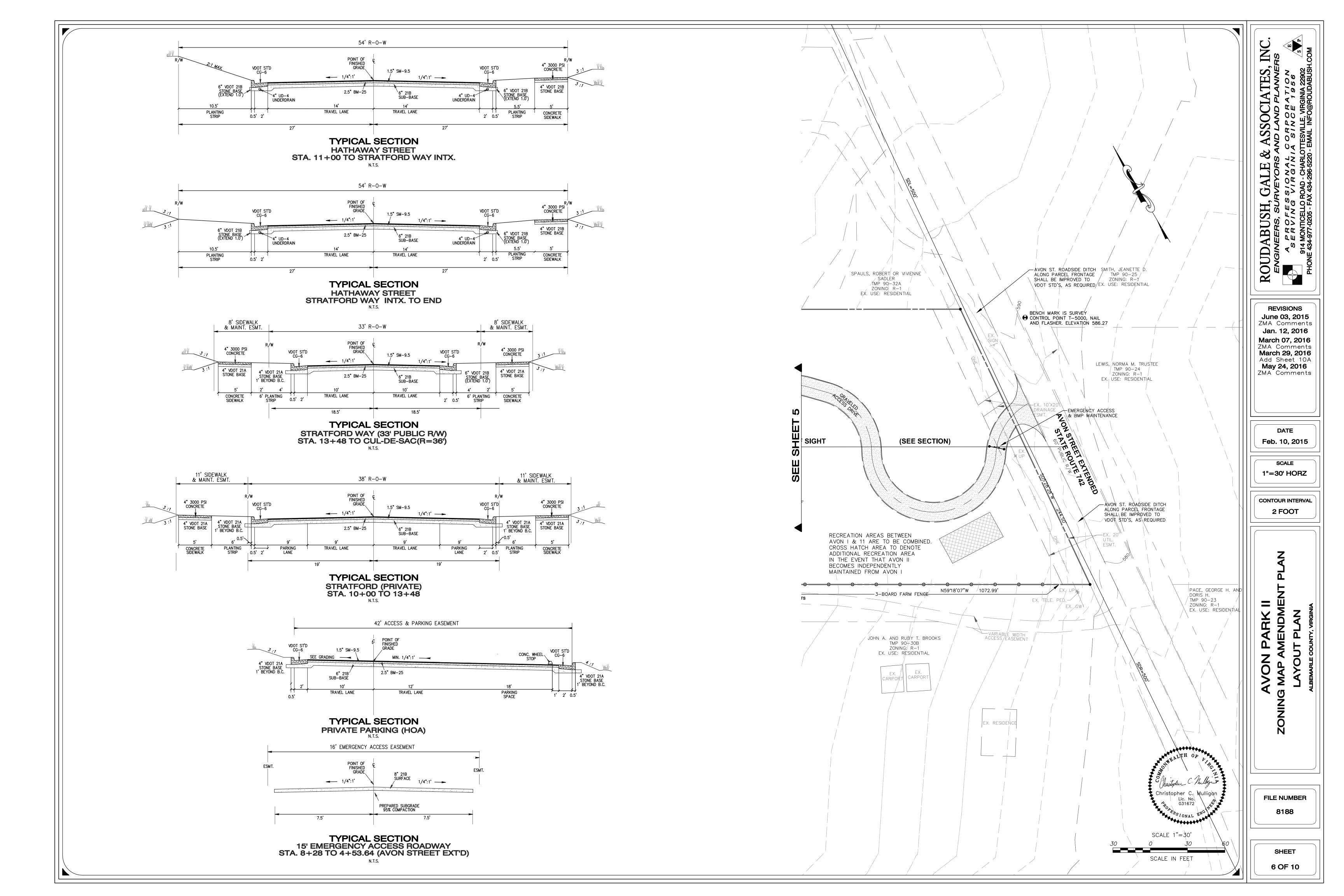
SCALE 1"=30' HORZ

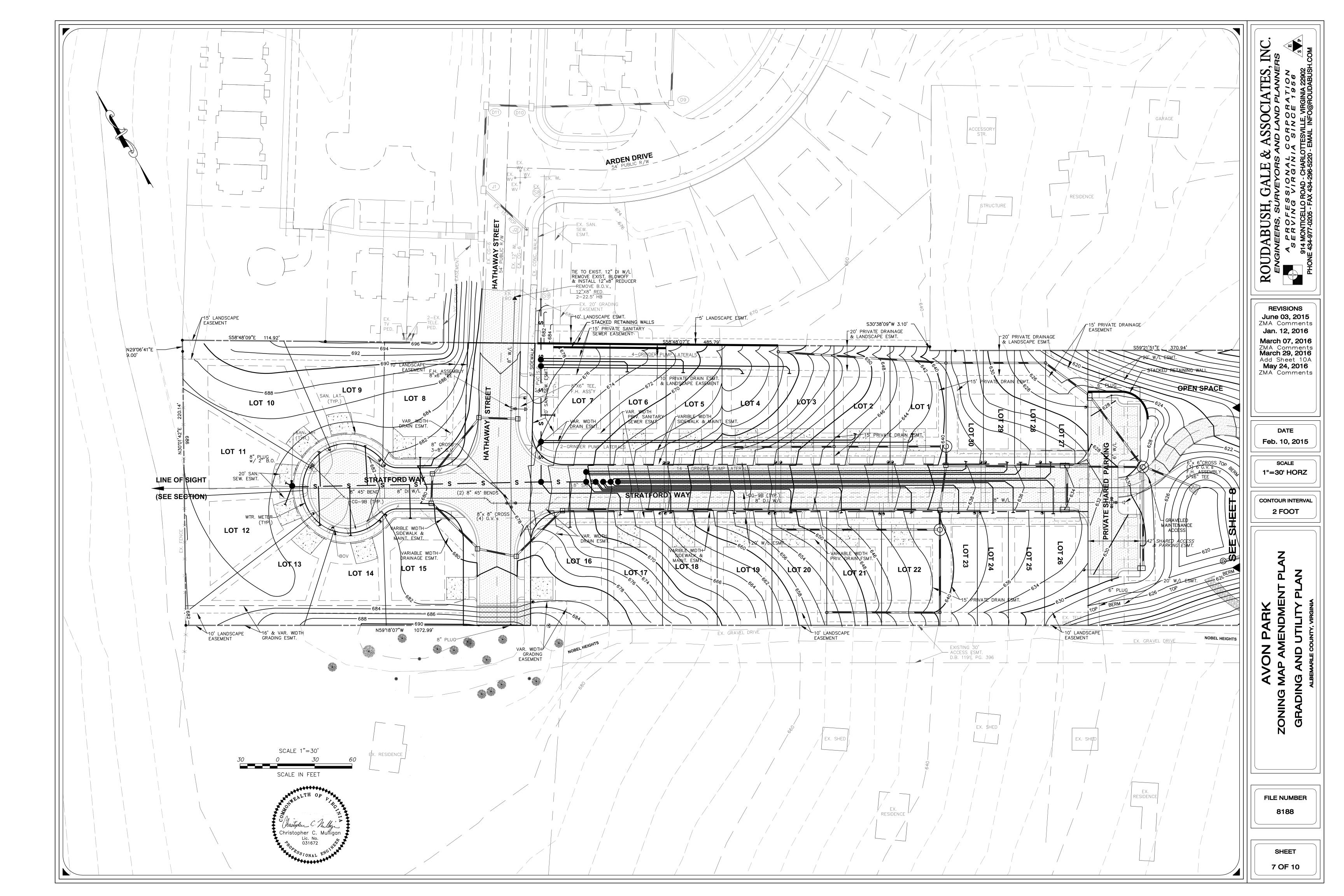
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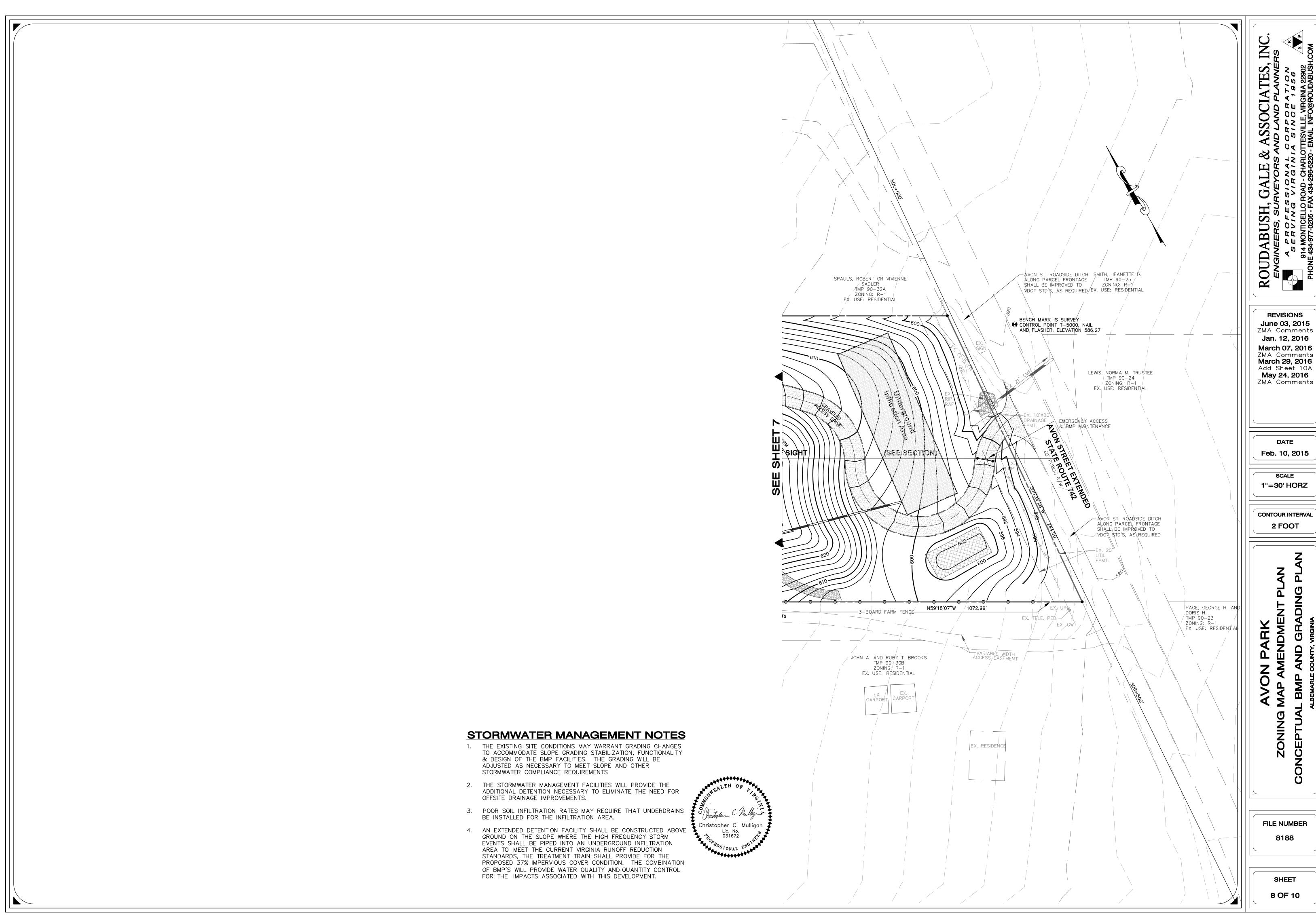
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FILE NUMBER

SHEET 5 OF 10







ROUD, ENGIN

May 24, 2016 ZMA Comments

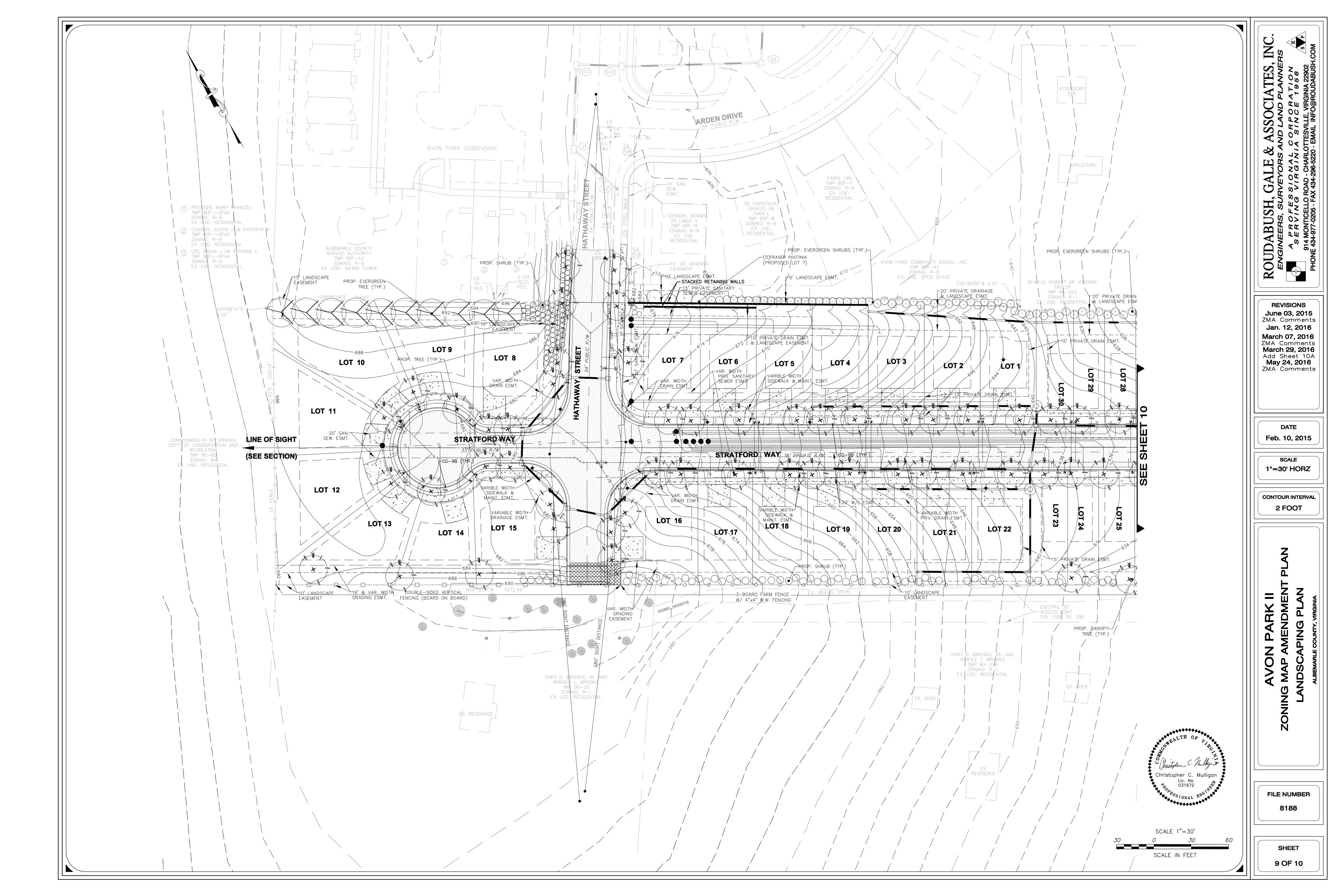
Feb. 10, 2015

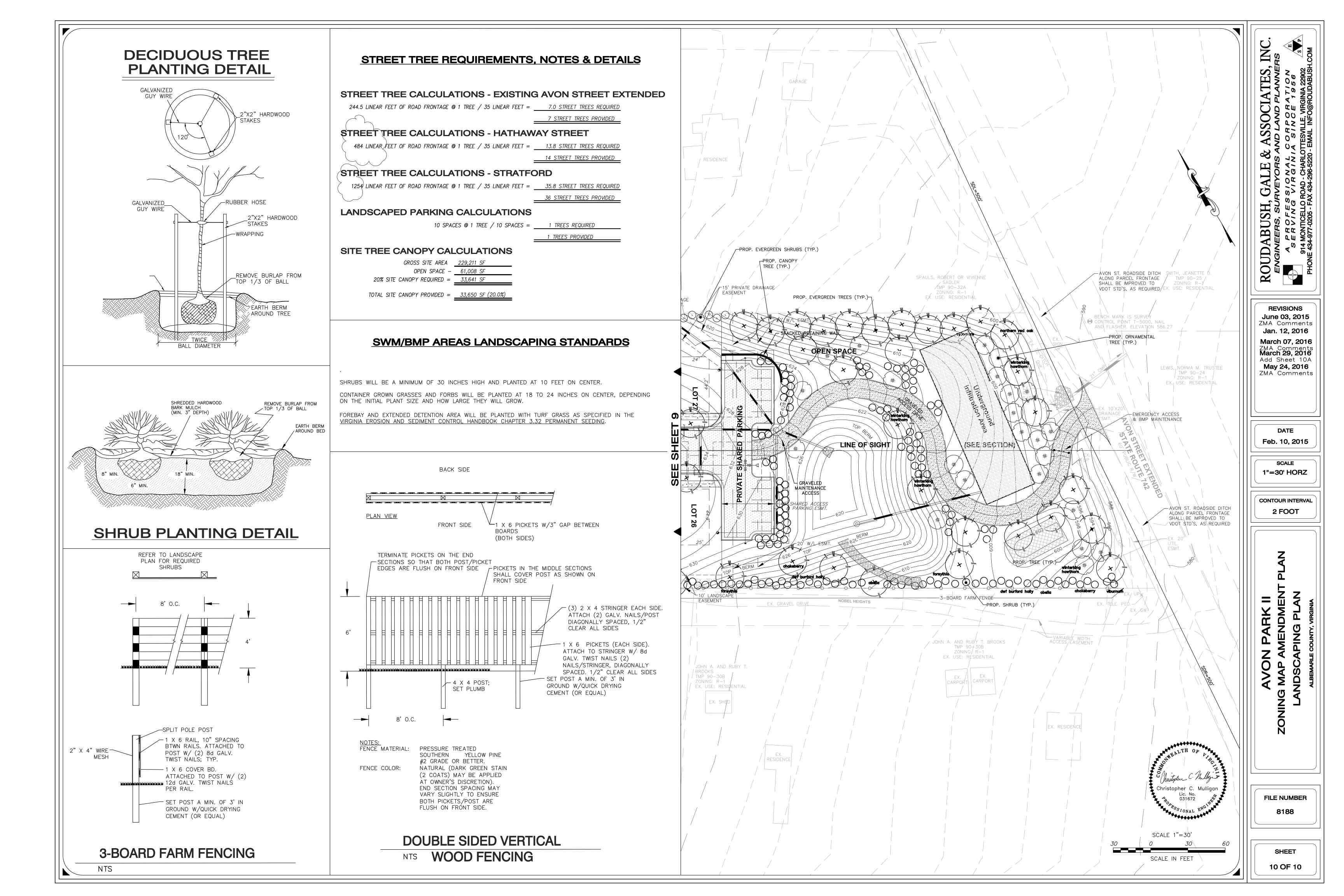
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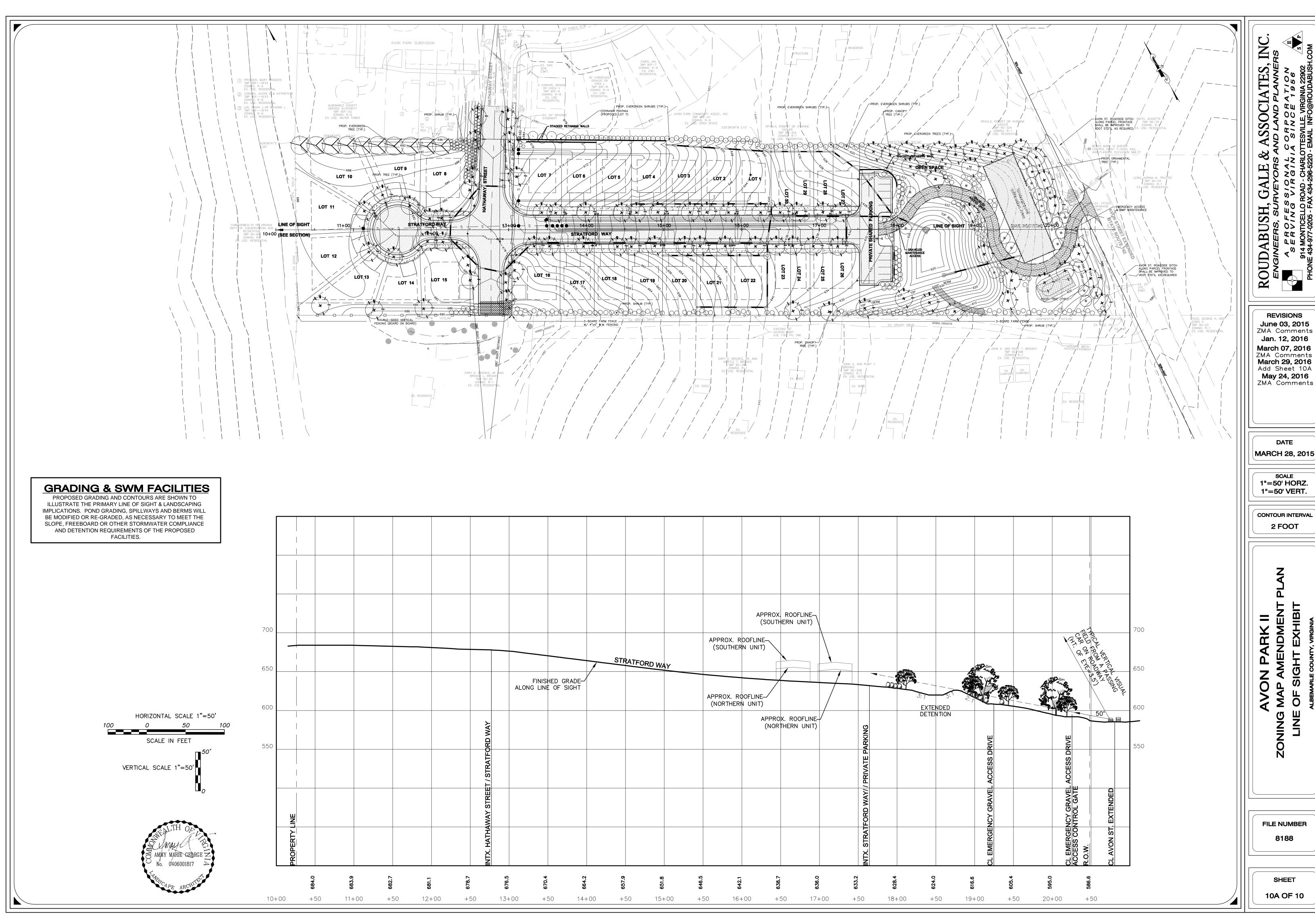
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FILE NUMBER

SHEET 8 OF 10







REVISIONS June 03, 2015 ZMA Comments Jan. 12, 2016 March 07, 2016 ZMA Comments **March 29, 2016** Add Sheet 10A

DATE MARCH 28, 2015

1"=50' HORZ. 1"=50' VERT.

CONTOUR INTERVAL

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FILE NUMBER

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SHEET 10A OF 10